



THE VOLUNTEER



VOL. 13 NO. 5

134TH AIR REFUELING WING

MAY 2013

Global Reach Through In-Flight Refueling

By Tech Sgt Jack M. West, 134 ARW Public Affairs

From the invention of powered-flight, one of the factors limiting how far an aircraft can fly is how much fuel it can hold. Since the early days of flight, aviators have had to know exactly how much fuel they have on board and how far they can go with that fuel.

The need for aircraft that could provide in-flight refueling was realized long before the creation of the United States Air Force in 1947.

This summer marks the 90th anniversary of the first in-flight refueling ever performed, in June 1923, between two U.S. Army Air Service de Havilland DH-4Bs.

Since that time, multiple aircraft have been used to perform in-flight refueling. Early on, the aircraft were single-engine aircraft that extended a hose to another aircraft to attempt refueling. Quite often, problems were encountered causing the crash of one or both aircraft.

Over the years, countries from around the world have attempted to create a tanker that could be depended on. The onset of World War II expedited the need for refueling aircraft that could help fighters and bombers reach around the world. Multiple aircraft were modified to serve as tankers as needed during the war, but the

first designated tanker in the Air Force inventory didn't come along until after the war had ended.

The first aircraft permanently utilized by the Air Force for in-flight refueling was the KC-97 Stratotanker. First used in 1950, the piston-driven aircraft was a modified C-97 cargo aircraft. It could reliably deliver a large amount of fuel, but it was a very slow aircraft. Jet-driven aircraft had been developed during World War II, and it soon became apparent that piston-driven

aircraft were going to be inefficient when it came to in-flight refueling. Although the Air Force began to phase them out in 1956 in favor of the KC-135, the KC-97 remained in the Air National Guard until 1978.

With World War II ended and the Korean Conflict going on, it was widely recognized that a large refueling aircraft that could keep up with the jet-driven fighters and bombers would be needed if the Air Force were to have a truly global reach. In June 1957, the initial production aircraft KC-135 Stratotanker was delivered to Castle Air Force Base in California.

The aircraft, powered by four turbofan engines mounted on 35-degree swept wings, was just what was

KC-135 (Cont. on Page 3)

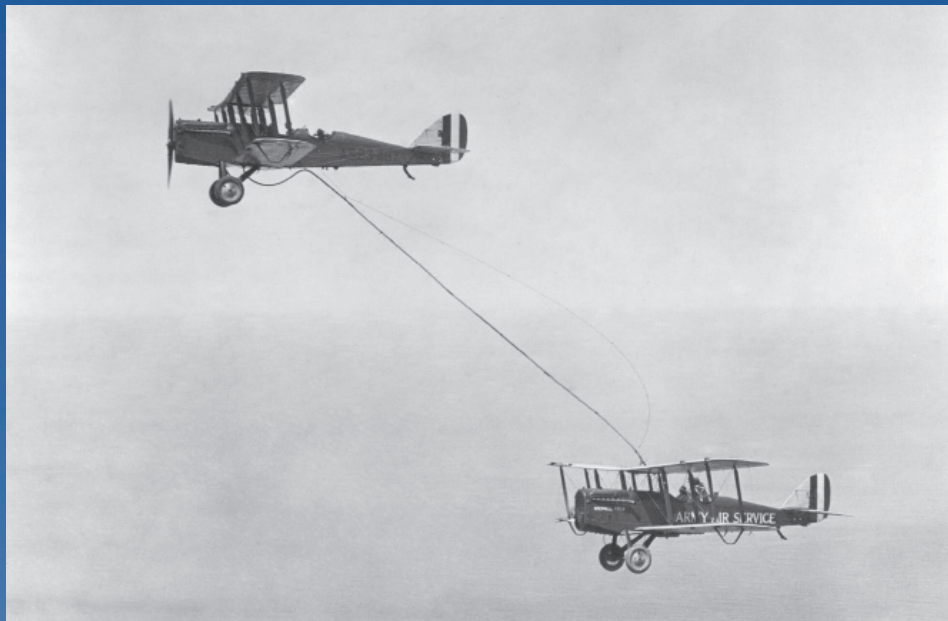


Photo of First mid-air refueling, June 1923. (US Air Force photo courtesy of Wikipedia)



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Wing Executive Officer**



“The Mission of the 134th Air Refueling Wing is to project Global Reach and Global Power in the interest of our National Defense by providing vital Air Refueling and Airlift for rapid deployment, sustained combat operations and effective training as directed by the Secretary of Defense.”

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Knowledge Operations**

Anti-Terrorism Force Protection Threat Levels Defined

IMMINENT

**WARNS OF A CREDIBLE, SPECIFIC, & IMPENDING TERRORIST
THREAT AGAINST THE U.S.**

ELEVATED

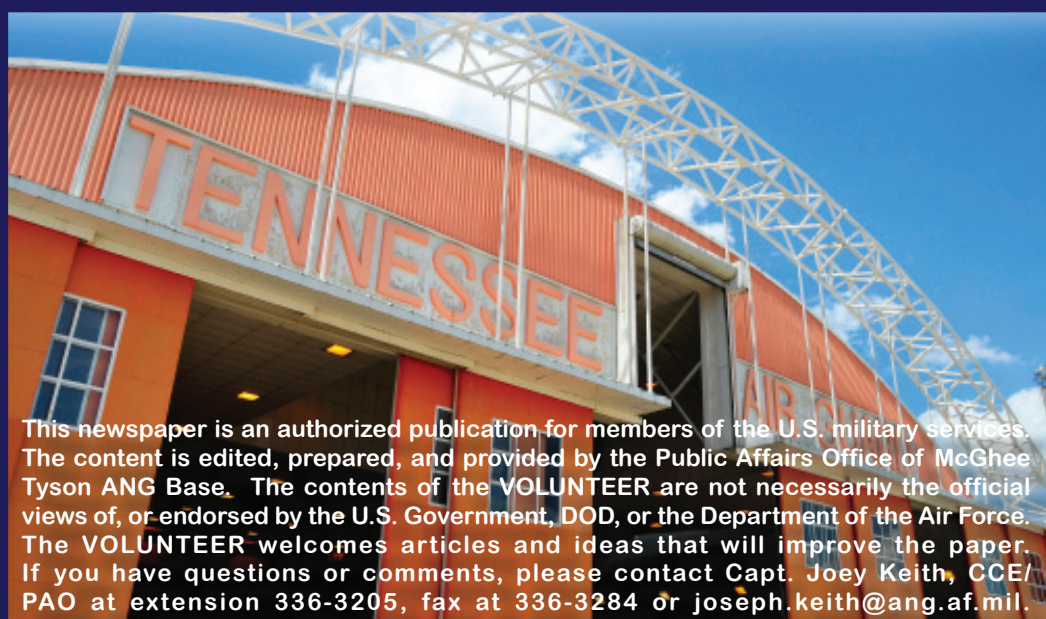
WARNS OF A CREDIBLE TERRORIST THREAT AGAINST THE U.S.

CURRENT LEVELS:

FPCON ALPHA --- INFOCON 3

REPORT SUSPICIOUS ACTIVITY!

**Call Extension 336-3274 or 911 - Your Call May Save Lives
THREAT LEVELS ARE SUBJECT TO CHANGE AT ANYTIME**



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needed to efficiently keep up with the jet-driven aircraft of the 1950s. Little did they know, the KC-135 would still be the backbone of the Air Force's fleet of refueling aircraft today, 56 years later.

Today, the KC-135 is a multi-mission aircraft performing aerial refueling missions to United States Air Force, Navy, and Marine Corps aircraft in addition to the aircraft of allied nations. It can also carry cargo and perform the aero-medical mission.

The KC-135's primary fuel transfer method is the boom on the rear of the aircraft. A special shuttlecock-shaped drogue attached to the boom may be used to refuel aircraft fitted with probes. Some KC-135s have been configured with the multipoint refueling system which consists of special pods mounted on the wing-tips. These KC-135's can refuel two aircraft simultaneously.

The KC-135 was designed by the Boeing Company using the basic design for the B-707 passenger plane. The Air Force purchased 732 of the aircraft that were delivered between 1957 and 1965.

The original aircraft model was the KC-135A. More than half of those original aircraft are still flying today with modified engines. The re-engined tankers are designated as either the KC-135R or KC-135T.

Through the years, the KC-135 has been modified to do other jobs ranging from flying command post missions to reconnaissance. Other designations of the KC-135 include a testing model designated the NKC-135A, the now retired KC-135E, and special mission aircraft including the OC-135B Open Skies, RC-135S Cobra Ball, RC-135U Combat Sent and the RC-135V/W Rivet

Joint.

The KC-135 is not the only tanker to be utilized by the Air Force in the modern era.

The KC-10 Extender entered service with the Air Force in 1981. Designed to ease the burden on existing aircraft, the KC-10 can carry almost twice as much fuel as the KC-135 while also carrying cargo and passengers.

Powered by three turbofan engines, the KC-10 is an efficient aircraft to refuel today's high-speed fighter and bomber aircraft.

While the KC-135 has made its way into the ranks of the active duty, Air National Guard and Air Force Reserve, the KC-10 has not. All 59 aircraft are utilized by the active duty Air Force.

The Air Force has begun planning to replace the aging KC-135 fleet. In 2011, Congress voted to move forward with the purchase of 179 Boeing KC-46 aircraft. The first aircraft, based on the Boeing 767 passenger aircraft, is expected to be built and flown in 2015. If all goes as scheduled, the first operational aircraft will be delivered to the Air Force in 2017 with the final aircraft being delivered in 2028. The sequester is not expected to have a significant impact on the program.

Information used for this article, and additional information about the aircraft described, can be found at:

<http://www.af.mil/information/factsheets/factsheet.asp?id=110>

<http://www.af.mil/news/story.asp?id=123343692>

<http://www.af.mil/information/factsheets/factsheet.asp?id=109>

UNDERGRADUATE PILOT TRAINEES SOUGHT

The 151st Air Refueling Squadron will be convening a selection board on for fiscal year 2013. The following criteria must be met to be considered for an Undergraduate Pilot Training position:

- Age 28 or younger
- Completed the AFOQT and TBAS test
- Able to pass a Class I Flying Physical
- Eligible to obtain a Top Secret security clearance
- A Bachelor's degree completed by Dec 2013
- Previous flying experience with a minimum private pilot certificate preferred
- Able to commit approximately 2 years for training



Prospective candidates are asked to follow the guidelines on the attached Pilot Announcement. Submit your application package no later than 17 May 2013.

Mail or turn in your package to MSgt Curtis LaRue (865) 336-3242.

Attn: MSgt Curtis LaRue
134 Briscoe Drive
McGhee Tyson ANG Base, TN 37777-6401

ANNOUNCEMENT OF VACANCY

MILITARY DRILL-STATUS OFFICER

POC: MSgt Curtis LaRue
865-366-3242
Curtis.larue@ang.af.mil

POSITION: Pilot KC-135R

OPENING DATE: 12 Apr 2013

AFSC: 11M3 GRADE: 2LT Thru Major

CLOSING DATE: 17 May 2013
1630 HOURS

UNIT: 151st Air Refueling Squadron

SPECIALTY DESCRIPTION: (SEE ATTACHED JOB DESCRIPTION FOR DETAILS)

1. GENERAL REQUIREMENTS AND PROCEDURES:

Prior Enlisted personnel, Commissioned Officer in the grade of Major or below (Active or Reserve) or non-prior service civilian may apply if qualified for commission as follows:

- **NON-COMMISSIONED APPLICANT AGE:** Due to the length of administrative processing involved in the commissioning process, applicants **must** be selected by age **28** and be no more than age **30** by the start date of the board's first available **Undergraduate Pilot Training Class (UPT)**. This will allow time for **non-prior commissioned** applicants to graduate from the Academy of Military Science as an ANG officer prior to attending UPT. **Prior-Commissioned Applicant** must have not exceeded rank-age ceiling. Call MSgt LaRue at 865-336-3242/DSN266-3242 for eligibility clarification.
- **MEDICAL REQUIREMENTS:** Applicants **must** pass a Flying Class 1 physical (Appointment ANG/AF Commissioning physical).
- **AIR FORCE OFFICER QUALIFYING TEST (AFOQT):** Applicants **must** have taken the AFOQT and qualify with a minimum score of **Verbal-15; Quantitative-10; Pilot 25; Navigator 10; Sum-50 (Sum is the minimum composite score required by adding both the scores of Pilot and Navigator.) Applicants must meet or exceed the minimum Pilot and/or Navigator scores to qualify for appointment and attendance of Undergraduate Pilot Training (UPT).**
- **EDUCATION:** Applicants **require** as a minimum a Bachelor's Degree from an educational institute listed in the current Accredited Institutions of Post Secondary Education. For entry into this specialty, undergraduate degree specializing in physical sciences, mathematics, administration, or management is desired. Applicant must have attained the degree or scheduled to attain the degree no later than **Dec.** of the year they are interviewed.
- **SPECIALTY QUALIFICATIONS:** Knowledge is mandatory of theory of flight, air navigation, meteorology, flying directives, aircraft operating procedures, and mission tactics. For entry into this specialty, completion of Air Force Undergraduate Pilot Training (UPT).
- **PILOT CANDIDATE SELECTION METHOD (PCSM):** Must complete Test of Basic Aviation Skills (TBAS) to obtain score. PCSM information website: <https://pcsm.aetc.af.mil/>
- **LETTER OF AGREEMENT:** Applicants **must** submit a letter of agreement to attend within one year (pending availability of school quotas) of commissioning (or assignment if prior commissioned officer) any initial formal training as listed in the Training Requirements section. **Applications are considered incomplete without this letter (see attachment 1).**
- **COMMISSION REINSTATEMENT:** Prior commissioned officers must be eligible to reinstate commission and retrain. Cross-component service officers will not be required to attend the Academy of Military Science.



2. HOW TO APPLY:

Each officer application package submitted **will** consist of the following (*but not all requested documents apply to all applicants*):

- **Cover letter** (State for which job(s) you are applying for.)
- **Air Force Qualifying Test (AFOQT) Scores:** (Scores used to determine if you meet the minimum score requirements for appointment consideration. It is your **responsibility** to take the test and include the test results with you package.)
- **Flying Qualification** (List all flying qualifications to include **Time Breakdown, Certificates, Awards, and Flying Experience**).
- **Officer Applicant Formal Training Agreement** (You agree to attend any formal training required). (**see attachment 1**)
- **Officer Applicant Questionnaire** (Do not leave any question unanswered.) (**see attachment 2**)
- **DD Form 214 (Certificate of Release or Discharge from Active Duty)** (Provide a copy of each DD Form 214 long form showing special additional information (i.e. Type, Character, Authority, and Narrative Reason for separation, etc.) (**prior service only**))
- **Resume must** reflect your experience and qualifications (to include civilian and military employment history) with detailed coverage of duties and responsibilities that relate to the job you are applying for. List both personal and professional references (include reliable phone numbers). Do not state, "References available upon request".
- **Record Review RIP/Personnel Information RIP** (Provides detailed military career information, (i.e., Individual Data, Duty Assignment Data, Qualification Data, Honors Data, etc.) (**prior service only**))
- **Letters of Recommendation** (It is optional to include letters of recommendation)
- **Pilot Candidate Selection Method (PCSM):** Must complete Test of Basic Aviation Skills (TBAS) to obtain score.

It is your **responsibility** to complete and return the above requested documents(s) **No Later Than closing date of this announcement**. Your package **will only** consist of the requested documents. **Keep your application simple and in the order listed above**. An incomplete package could make the difference in the officer selection process. Submit officer package(s) to 134 FSS, 134 Briscoe Dr, MTAB TN 37777, ATTN: MSgt Curtis LaRue. **The Wing Commander or Vice Commander will review the OG recommendations and forward his/her recommendations to the Assistant AG/Air. The AG/Air will be the final approval authority for the selection of the officer candidate.**

3. TRAINING REQUIREMENTS

- Medical Flight Screening (MFS)/ Wright Patterson AFB, Ohio, 5 days : **Must** bring medical records
- Initial Flight Screening (IFS)/ Pueblo, Co, 4 weeks:: All pilot candidates who do not possess a Private Pilots License will attend IFS prior to entering UPT
- Non-prior commission officer selectee: **Must** attend the Academy of Military Science (AMS), 6 weeks, at Maxwell AFB Montgomery, Alabama
- Undergraduate Pilot Training/ Columbus AFB, MS or Vance AFB, OK or Laughlin AFB, TX, 1 year.
- Initial Pilot Qualification/ Altus AFB, OK, 22 weeks.
- Combat Survival and Water Survival Training/ Fairchild AFB, WA, 3 1/2 weeks.
- Seasoning Training/ Home Station, 45 days (**Mandatory Training**)

4. PRE-REQUISITES: None

5. SPECIAL REQUIREMENTS: Upon selection: Selectees must agree to live within 50 miles of McGhee Tyson ANGB upon Formal training completion and throughout their career.

In accordance with applicable directive, selection for this position vacancy will be made on a **BEST QUALIFIED** basis without regard to race, national origin, ethnic background, religion, color, or gender. Age restrictions in accordance with federal statues apply. For more information, call MSgt Curtis LaRue at (865) 336-3242/DSN 266-3242.



The M50 Mask (JSPGM) Storage-Long Term

Master Sgt Chris King, 134 CES Emergency Management

The M50 JSPGM mask should be stored appropriately when use is not immediate or planned for long term. Below are the proper guideline procedures for storage:

Remove the M61 filters and place them in the plastic filter cases issued and store them in the Individual Equipment Carrier.

Remove the Microphone (if issued) and store it in the elastic tab inside the bottom of the mask carrier.

The clear Outsert is stored on the mask to protect the lenses.

Clean your mask properly and remember to use only the micro fiber cloth issued with the mask, that the clear Outsert came in, to wipe the lens or Outsert.

Once the mask is completely dry, correctly put the white plastic Faceform inside the mask.

Pull out the inner Protection Sleeve of the mask carrier out and seat the mask inside facing away from the body or to the outside of the carrier.


Fold or roll the Protection Sleeve down and seal the mask inside the carrier by closing the Carrier Flap Tab. The mask is stored for a long term up to the next inspection period.

The availability and issue of the M50 mask has been greatly beneficial to every airman. Proper care of this important piece of equipment will assure many years of use and the protection it offers.

Additional information is available in the Airman's Manual AFPAM 10-100 and the Operator Cards issued with the mask.

Information provided in the chart (right) is an overall summary of information useful to copy and keep.

Joint Service General Purpose Mask (JSGPM)



WARNING

DO NOT use tissue paper or a paper towel to remove dirt or moisture from outlet valve cover assembly, outlet disk valve, or outlet valve seat. Paper may break up and lodge in outlet valve area causing leakage.

It is important to properly clean your mask. A dirty mask will compromise its ability to provide maximum protection.

CAUTION

DO NOT use high pressure water to clean mask, it may damage components.

Care must be taken not to scratch the eye lens and outserts when handling the mask assembly.

This mask is not effective against industrial chemicals such as Ammonia, Chlorine, and Carbon Monoxide fumes. The mask is not effective in confined spaces when there is not enough oxygen to support life. The mask does not supply or produce oxygen.

Installation of the M61 Filter

Pick up the filter with the side tabs facing toward the bottom

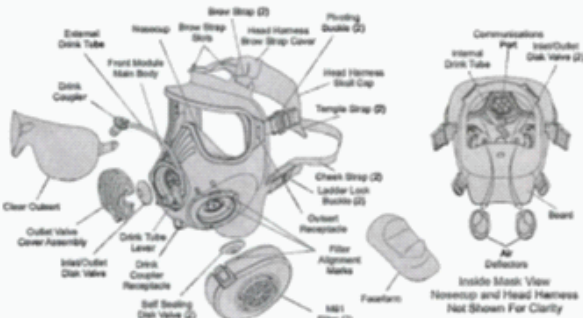
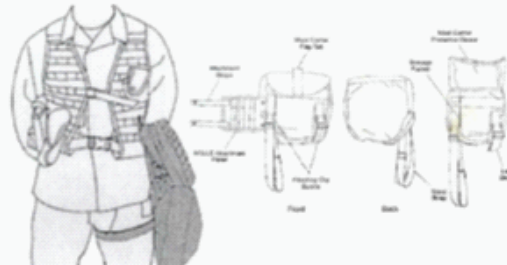
Align the single filter alignment tab with the double alignment marking on the face piece assembly ensuring the filter is directly over the self-sealing disk valve and press until the filter is snug against the mask

While pressing the filter to the filter mount, turn it toward the back until the single alignment tab is aligned with the single alignment marking on the face piece assembly and the tabs click

Removal

Grip the filter side tabs on the M61 Filter and squeeze inward

Twist the filter towards the front of the mask, and lift it from the filter mount

Light Cleaning

- Dip outsert pouch in warm soapy water and wring pouch almost dry
- Clean:
 - Mask assembly
 - Outlet disk valve
 - Inlet disk valves
 - Self-sealing disk valves
 - Head harness
- Rinse by dipping outsert pouch in warm clear water wring outsert pouch almost dry and wipe mask assembly with clean water
- Allow to air dry

Light cleaning after normal operations

Heavy Cleaning

- Immerse the face piece, front module, outlet disk valve, inlet disk valves, self sealing disk valves, head harness and filter mount air deflectors
- Agitate until all sand and dirt has been removed
- Rinse in clear, warm water
- Dry face piece, front module cover, outlet disk valve, inlet disk valves, self sealing disk valves, head harness, and filter mount air deflectors with outsert pouch; allow to air dry
- Clean eye lens and outserts with outsert pouch

Inspection frequency

- When issued to individual
- Prior to each use of the mask (by user)
- Monthly (by user)
- Every 6-months (Field level PMCS by LRS)
- Upon turn-in

National Stock Numbers (NSN)

- Small 4240-01-512-4431
- Medium 4240-01-512-4434
- Large 4240-01-512-4437

Issued on	Initial	Inspection due
1 Sept 10	JMS	1 Oct 10
1 Oct 10	JMS	1 Nov 10
10 Oct 10	JMS	10 Nov 10
		15 Nov 10

FOR PARTS AND ITEM DESCRIPTION: **NSN 4240-01-512-4437**

M50 JSGPM

Large

ISSUE NUMBER: 102430007R108 / AV00948L06

CONTRACT NUMBER: N/A

QUANTITY: 1

REMARKS: A real-world M61 filter set was installed on: Lot number:

SERVICEABLE TAG - MATERIAL

SEE REVERSE

17 CES/CEX

Amn Ian M Snuffy

1 Sept 2010

Congressional Medal of Honor Society Convention Coming to Knoxville

The Medal of Honor is the highest award for valor in action against an enemy force which can be bestowed upon an individual serving in the Armed Services of the United States. Generally presented to its recipient by the President of the United States of America in the name of Congress.

The Congressional Medal of Honor Society will hold its 2014 convention in Knoxville honoring the 80 living Medal of Honor recipients. Beginning next month, articles about the recipients of the Congressional Medal of Honor will be highlighted in the monthly Volunteer newspapers.



The 3 Present Day Variations of the Medal Of Honor

MTAB ANTITERRORISM QUICK REFERENCE GUIDE

REPORT SUSPICIOUS ACTIVITY IMMEDIATELY!

Signs of Terrorism

1. **Surveillance:** Someone recording or monitoring activities using cameras (either still or video), note taking, drawing diagrams, annotating on or creating maps, and using binoculars or other vision-enhancing devices. An element of this activity could also include mapping out routes and determining the timing of traffic lights and flow.
2. **Elicitation:** People attempting to obtain information about you or your military unit. These attempts may be made by mail, fax, telephone, e-mail or in person and may seem like totally innocent questions about what you do and where you work.
3. **Tests of Security:** Any attempt to measure reaction times and actions by police. A test of security can be disguised as a simple mistake such as a vehicle approaching a security barrier and then turning around or other attempts to circumvent access control procedures. Be particularly aware of anyone attempting to access the installation with invalid ID.
4. **Acquiring Supplies:** Purchasing or stealing police and military uniforms, emergency responder type vehicles (such as police cars and ambulances), building passes and other identification media or the equipment to manufacture them.
5. **Dry Run or Practice:** People, items (i.e. a "forgotten backpack") or vehicles appear to have been purposely placed in a particular position or area.
6. **Actual Deployment of People and Assets:** People and supplies getting into position to conduct an attack. This is the last chance to alert authorities before the attack occurs.
7. **Suspicious Persons:** People who don't seem to belong or fit in the surrounding environment, especially if involved in any of the above listed activities.

Junior Enlisted Advisory Association: Opportunities Abound

By Tech. Sgt. Jack West, 134 Public Affairs

During March drill, I had the opportunity to talk with Staff Sgt. Laura Morgan, an accounting analyst with the 134th Comptroller Flight, and also the president of the Junior Enlisted Advisory Association, or JEAA, since December. Morgan shared some of her vision for the future of the JEAA in addition to telling about some of the upcoming activities they have planned.

JW: What is the JEAA?

LM: JEAA is the Junior Enlisted Advisory Association.

JW: What is the purpose of the JEAA?

LM: I looked all over. I looked on the Internet. I couldn't really find much about it. I determined JEAA is anything we wanted to make it. Over the last few months, we've decided we want to make it a motivating tool, a leadership opportunity for the members. It's for camaraderie, where people can go together for a good time, but get opportunities they normally don't get, especially as being a traditional.

JW: What are some projects the JEAA is involved with?

LM: In the past they've done all kinds of things. They've done Top Wrench, fund-raisers to bring in money for Santa Cops, and can collections.

JW: What kind of can collections do you do?

LM: We have a trailer the Chiefs Council gave us. People can bring aluminum cans to be recycled and dump them in the trailer parked outside of the Armed Forces Club. When it fills up, we go cash them in. That money all goes toward the scholarships we give out.

JW: Tell me more about the scholarships.

LM: We offer \$500 scholarships, two in the fall and two in the spring. Just write a letter of what your goals are, what you've been doing, and why you deserve this. The scholarships can be applied toward an associate's or bachelor's degree. You don't have to be a member of JEAA to win, but you do have to be a Tech. Sgt. or below.

JW: You've talked about some of the projects JEAA has been involved in previously. What projects do you have coming up?

LM: We have a ton of projects coming up in the near future. In June, we'll be the lead for the ice cream social. This is our first base-wide project. It might end up being more of a favorite summer dessert more than just ice cream. We're not about it being a competition; it might be more of just a social event. Then for May, we've had several people in our membership have to stay at Ronald McDonald House because their children were sick. It's very dear to them, so it's definitely something that we want to contribute to. We're going to send out laundry baskets to every section. The sections have May and June to bring in anything off a list we'll provide to them to fill those baskets. Things like cleaning supplies, household supplies, foods and coffee. We're thinking about making that a competition to see which section can bring in the most stuff. We'll possibly do pie in the face at the Family Day in September. We're going to do the chili cook-off again in November, and in December we decorate the base Christmas tree.

JW: When does the JEAA usually meet?

LM: We normally meet on the Saturday of drill in the HQ conference room, usually about 14:30. Listen for the announcements.

JW: Who can be a member of the JEAA?

LM: Anyone who is a Tech. Sgt. and below can be a member.

JW: Are there any dues to be a member?

LM: No, there are no dues.

JW: Who can people contact for more information about becoming a member or about any of the projects you have planned?

LM: Myself, Tech Sgt Jan Bridget, the vice president, Staff Sgt Jessica Kreis, the treasurer, or Tech Sgt Lindsey Watson-Kirwin, the secretary. They can also contact their section representatives.

JW: What would you say to somebody who's thinking about becoming a member?

LM: This is an opportunity to set yourself a little bit higher and to show that you can be better. I highly recommend this. I wish I was more involved in this as an Airman. I know that if I had been more involved in this, I would have known a lot more people and I'd have been more connected.

Staff Sgt. Morgan can be reached at 336-4236 or via email at laura.morgan@ang.af.mil.



INFORMATION ASSURANCE

Information Assurance Awareness: A Risk to One is a Risk to All!

Submitted by Staff Sgt. Adam Huskey, 134 Communications Flt



Management Internal Control Toolset (MICT) supersedes AF Form 4160 (IAAP)

The Air Force has implemented the Management Internal Control Toolset or MICT as of Jan 1, 2013. It is the new system of record for organizational self-assessment and checklist management requirements.

Designed by the AF Reserve to evaluate organizational effectiveness and ensure compliance with directives, MICT employs tools to self-evaluate and address deficiencies in self-assessment programs.

MICT is a web-based program available through AF Portal that has the ability to track compliance with various initiatives and hold digital copies of supporting documentation.

Eventually, most members will have a program that requires access to MICT. Luckily, we do not have to guess what applies to us. The checklists are assigned by the unit administrators. When that member gets ready to do a self-inspection, they will see only the checklists assigned to them. Upon opening any of their assigned checklists, there are radio buttons and comment blocks to make the paperless process as simple as possible.

For more information on our way forward for Self-Assessment, See:

<http://www.citamn.afrc.af.mil/features/story.asp?id=123287059>

Tips to avoid data spillage:

- **Check all documents for classification level**
- **Know the different types of networks**
NIPRNet - for unclassified data
SIPRNet – for classified (secret) data
- **Be aware of which network you are on**
- **Label all files, removable media, and subject headers**
- **If a spillage occurs, notify your security POC**
- **When storing or transmitting sensitive information, including PII:**
Encrypt before storing on mobile devices or transmitting
E-mail with caution
Store on authorized system
- **Never transmit, store, or process on a non-sensitive system**

Tips on E-Mail Usage:

- **Use online sites to confirm or expose potential e-mail hoaxes**
- **Do not forward e-mail hoaxes**
- **View e-mail in plain text**
- **Use caution when opening e-mail**
- **All attachments should be scanned**
- **Delete e-mail from senders you do not know (before opening)**
- **Turn off preview pane**

Email should not:

Adversely affect performance
Reflect poorly on the Government

- **Do not use e-mail to:**
Sell anything
Send chain letters
Send offensive letters
- **Do not send:**
Mass e-mails
Jokes or Pictures
Inspirational stories
- **Avoid using Reply All**

**** Please call us in the IA office if we can ever be of assistance!**

Information Assurance POCs:
SSgt Huskey, 336-4936
MSgt Jennings, 336-4924



Submitted by Senior Master Sgt Richard Parker, 134 ARW Human Resource Advisor



Origins of APA Heritage Month

A national celebration established in 1977

by Ricco Villanueva Siasoco

May is Asian Pacific American (APA) Heritage Month—a celebration of Asians and Pacific Islanders in the United States. Much like Black History and Women's History celebrations, APA Heritage Month originated in a congressional bill.

Congressional Bills Establish Celebration

In June 1977, Representatives Frank Horton of New York and Norman Y. Mineta of California introduced a House resolution that called upon the president to proclaim the first ten days of May as Asian/Pacific Heritage Week. The following month, senators Daniel Inouye and Spark Matsunaga introduced a similar bill in the Senate. Both were passed.

On October 5, 1978, President Jimmy Carter signed a joint resolution designating the annual celebration.

APA Becomes Month long Celebration

In May 1990, the holiday was expanded further when President George H. W. Bush designated May to be Asian Pacific American Heritage Month. May was chosen to commemorate the immigration of the first Japanese to the United States on May 7, 1843, and to mark the anniversary of the completion of the transcontinental railroad on May 10, 1869. The majority of the workers who laid the tracks were Chinese immigrants.

Asian Pacific American Heritage Month is celebrated with community festivals, government-sponsored activities, and educational activities for students. This year's theme is "Leadership to Meet the Challenge of a Changing World."

Stress Tips from Elizabeth, Your Wing Director of Psychological Health

Reap the Reward. It is very important that after a long preparation and endurance, you stop and reap the reward. Savoring the moment of victory reinforces all the hard work you put forth. So, as the preparation and endurance of the ORI is behind you, enjoy the Reward – you have earned it!

Until next time, Happy Psychological Health



Airmen from the Joint Base Langley-Eustis Airman Leadership School in Virginia graduated March 28, 2013. Since graduating, the Airmen are now trained to supervise other Airmen. (U.S. Air Force photo/Staff Sgt. Jared Denton)



MEDICAL SCHEDULE

SATURDAY UTA

0815-1130 HOURS

0830-0850 HOURS

0830-1130 HOURS

0830-1130 HOURS

0900-1000 HOURS

0900-1100 HOURS

1000-1100 HOURS

1000-1030 HOURS

PHAS

FITNESS FOR DUTY EVALUATIONS

IMMUNIZATIONS

QNFT TESTING

DEPLOYMENT HEALTH ASSESSMENTS

ALL DEPLOYMENT PROCESSING/ANAM TESTING (TBDD)

FITNESS TESTING EVALS

NEW ACCESSION ORIENTATION (MDG TRAINING ROOM)

SUNDAY UTA

CLOSED FOR TRAINING WITH THE EXCEPTION OF THE FOLLOWING TIMES:

0845-0900 HOURS

1000-1100 HOURS

INSERTS

FITNESS FOR DUTY EVALUATIONS

BALLISTIC EYEWEAR & M-50 GAS MASK

PLEASE REMEMBER TO REPORT ALL MEDICATIONS THAT YOU ARE TAKING TO THE MED GROUP ASAP

NOTE 1: INDIVIDUALS REQUIRING LIPID TESTS MUST FAST FOR 14 HOURS PRIOR TO PHYSICAL EXAMINATION.

NOTE 2: INDIVIDUALS ARE NOT TO CONSUME ANY ALCOHOL FOR AT LEAST 72 HOURS PRIOR TO PHYSICAL EXAMINATION.

NOTE 3: IF YOU WEAR GLASSES YOU MUST BRING THEM WITH YOU FOR PHYSICAL. PLEASE DO NOT WEAR CONTACT LENSES FOR EXAM.

NOTE 4: IF YOU ARE DEPLOYING, PLEASE CHECK WITH THE CLINIC WELL IN ADVANCE OF DEPARTURE DATE FOR IMMUNIZATION REQUIREMENTS.

NOTE 5: DNA TESTING IS CONDUCTED FROM 0830 - 1100 HOURS OF SATURDAY UTA. PLEASE HAVE YOUR PERSONNEL REPORT AT SCHEDULED TIME.

ANG IP ALERT

Advertisers are getting more creative with technology.

In an Oct 2012 Entertainment Weekly had a “smartphone-like Android device” in the magazine. This device contained a 3G modem, some degree of voice connectivity, a full-sized battery, USB Port and a partially built QWERTY keyboard.



More recently FORBES magazine released an advertisement with a wireless hotspot for up to 5 devices on it (see page 2).

With the rapid advancement of technology, EVERYONE needs to be “on the look out”. We need to ensure these devices are not making it to our areas processing classified and our Open Storage Areas to include SCIFs.

It is recommended ALL Open Storage areas conduct RANDOM inspections of items entering and leaving the area to ensure these advanced technologies do not become a threat to our protection of classified.



RECRUITER'S CORNER

Submitted by the 134 ARW Recruiting Office

Student Flight – Listen Up!

***Pre-BMT class – You will report to the FSS breakroom in the Headquarters building for roll call on **SATURDAY** of each drill at **1300**. This is a **mandatory** class (as directed by Col. Cauthen) until you ship out to BMT. You must report to roll call in order to get paid. Any absence must be cleared prior to roll-call through the recruiting office supervisor, Master Sgt. Curtis LaRue.

***In-processing checklist - Saturday at 1230 of your first drill weekend you are required to report to the Public Affairs/Multimedia Photo Studio (in the headquarters building) to have your **portrait** taken. This photo is **mandatory** in order to complete your in-processing checklist.

***Security Clearance Process - All new enlistees please ensure you have logged onto www.opm.gov/e-quip, to initiate your clearance. Once you have logged on, you have 60 DAYS to complete this. This is a **mandatory** item. **Failure to complete on time will result in a counseling session with the Force Support Squadron Commander, Lt. Col. Dean Thiele.** Note: answer 'unknown' the first time and only the first time you answer the question 'place of birth.'

To Contact the Recruiters: Call DSN 266 or (865) 336-3242, 3257, 3258, or 3262; or go to: <http://www.134arw.ang.af.mil/careers/index.asp>

NEW AIR GUARD MEMBERS



**A1C Tyler Isaacs
134 LRS**



**A1C Caleb Selvage
134 MXS**



**A1C Keenan Tate
134 CES**



**AB Quinton Boyd
134 CES**



**AB Lonnie Collett
134 LRS**



**AMN Steven Jagneauz
134 MXS**



**SrA Patrick Deen
134 SFS**



**SrA Frank Dell
134 LRS**



**SSgt Nathaniel Boehlke
134 LRS**



**SSgt Robert Branson
119 CACS**



**SSgt Timothy Cummings
134 MXS**



**SSgt Randy Davis
134 CES**



**SSgt Jody Hendricks
134 OPS**



AROUND BASE

REMINDER

DUE TO THE POST-ORI TAILGATE PARTY ON SATURDAY AFTERNOON THE PUBLIC AFFAIRS STUDIO CUSTOMER SERVICE HOURS WILL BE MOVED TO 0930-1130 FOR MAY DRILL



ROCKY TOP DINING FACILITY

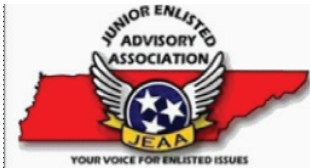
Saturday:

BAKED FISH
5 SPICE CHICKEN
BROWN RICE
MASHED POTATOES
YELLOW SQUASH
FRIED CABBAGE
GRAVY
ASST FRUITS & BREADS
SALAD BAR
SOUP

Sunday:

BEEF BULGOGI
SWEET & SOUR PORK
STEAMED RICE
FRIED RICE
VEGETABLE STIR FRY
EGG ROLLS
GRAVY
ASST FRUITS & BREADS
SALAD BAR
SOUP

JEAA



Due to the tailgate party the Junior Enlisted Advisory Association meeting will be cancelled for May drill. Meetings will resume June drill. All E-1s through E-6s are encouraged to attend!

Leadership Blount Tours Base

What is it like to be an airman for the day? Some Blount County civilian leadership got to find out during a base tour Apr 22. Approx. 17 individuals got the opportunity to tour a KC-135R Stratotanker aircraft, learn about aircrew survival in an emergency situation, and experience what its like to carry the full load of battle gear that a Security Forces airman carries on a typical deployment. (US Air Force photos by Master Sgt. Kendra Owenby, 134 ARW Public Affairs)

